

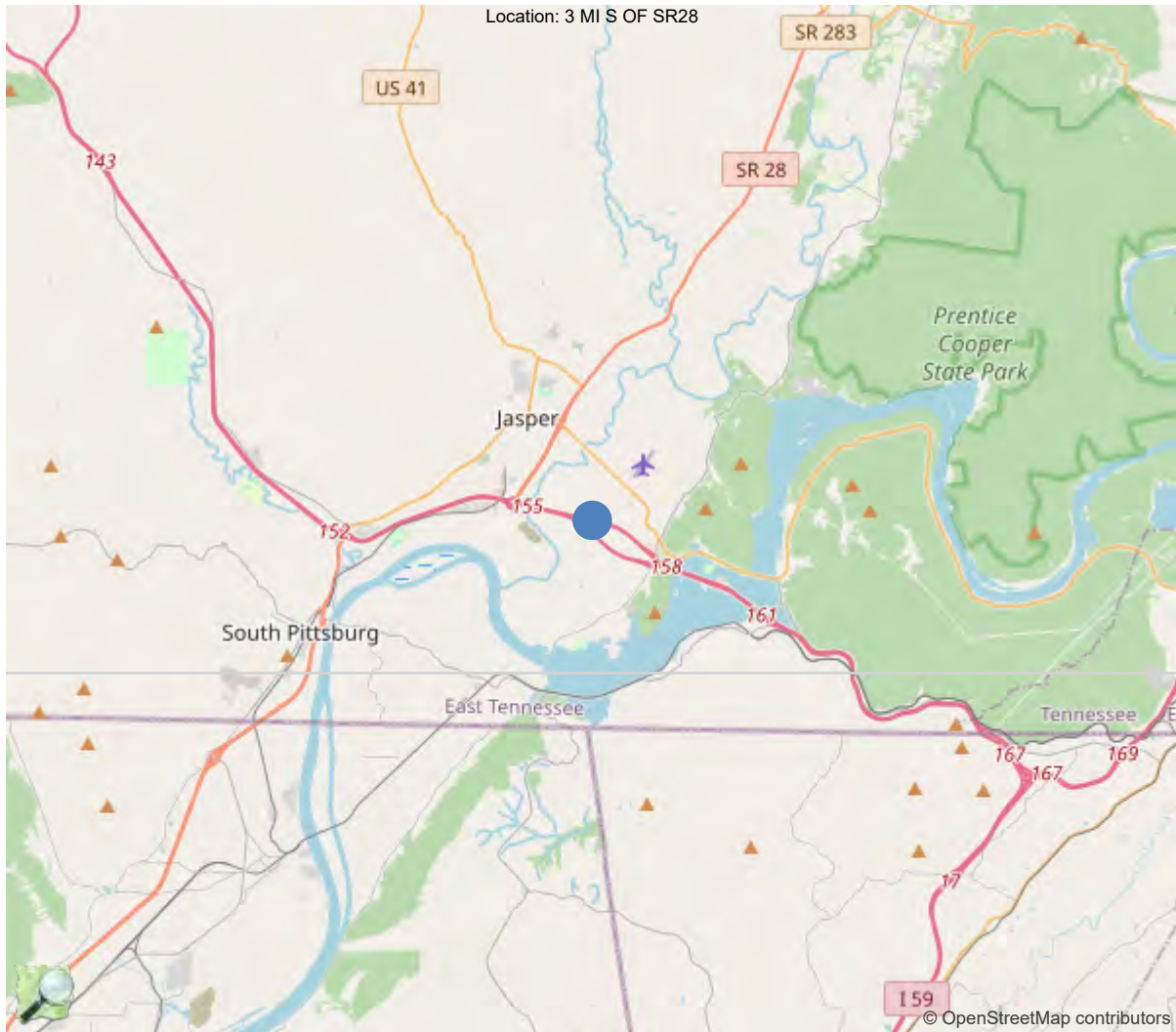


Latitude:35.04398, Longitude:-85.60290

Region 02, 58 - Marion County

Team Leader: Derek Yates

Inspectors: Anthony Pack



I-24 Crossing I-24 WB / SHELLMOUND RD.

35.04398, -85.60290

Routine Bridge Inspection Report

| Marion County | |
|-------------------|--|
| Federal ID | 58I00240069 |
| Location | 58-I0024-22.65 |
| Description | Interstate 24 Westbound Lanes over Shellmound Road, I-24 Milepost 157.16 |
| GPS Coordinates | 35.043983, -85.602900 |
| Date | 7/16/2024 |
| Overall Condition | Fair |



Maintenance Recommendations

525 - Repair List # 2 523 - Repair List Add Date 9/13/2013 524 - Repair List Revise Date 7/13/2022

| Date Added | Recommendation | Priority |
|------------|---|----------|
| 08/26/2008 | APPROACH GUARDRAILS ARE SUBSTANDARD | |
| 08/26/2008 | APPROACH GUARDRAIL TERMINALS ARE SUBSTANDARD | |
| 09/17/2002 | UNDERPASS SUBSTRUCTURE PROTECTION GUARDRAILS ARE NON-EXISTENT | |
| 07/13/2022 | REPAIR CONCRETE GIRDERS "A", "B" AND "C" IN SPAN #2 | 2 |

90 - LAST INSPECTION DATE 07/16/2024

10 - MIN. V.C. OVER DECK
(ROADWAY + SHOULDERS) 99.99 FT.

520 - MIN. V.C. OVER DECK
(EXCLUDES SHOULDERS) 99.99 FT.

36 - TRAFFIC SAFETY FEATURES

| Br. Rail | Trans. | Appr. Rail | Terminal | SPEED LIM. |
|----------|--------|------------|----------|------------|
| 1 | 0 | 0 | N | 70 |

41 - STRC OPEN/CLOSED/POSTED A

58 - DECK 6

59 - SUPERSTRUCTURE 5

60 - SUBSTRUCTURE 7

61 - CHANL/CHANL PROTECTION N

62 - CULVERT AND RETAIN WALL N

71 - WATERWAY ADEQUACY N

72 - APPROACH RDWY ALIGNMENT 8

521 - OVERALL CONDITION 2 - Fair

| 16 - LATITUDE | 17 - LONGITUDE |
|---------------|----------------|
| 35.043983 | -85.602900 |



TEAM LEADER SIGNATURE

N NOT APPLICABLE

9 EXCELLENT CONDITION

8 VERY GOOD CONDITION - NO PROBLEMS NOTED.

7 GOOD CONDITION - SOME MINOR PROBLEMS.

6 SATISFACTORY CONDITION - MINOR DETERIORATION OF STRUCTURAL ELEMENTS.

5 FAIR CONDITION - ALL PRIMARY STRUCTURAL ELEMENTS ARE SOUND BUT MAY HAVE MINOR SECTION LOSS, CRACKING, SPALLING OR SCOUR.

4 POOR CONDITION - ADVANCED SECTION LOSS, DETERIORATION, SPALLING OR SCOUR.

3 SERIOUS CONDITION - LOSS OF SECTION, DETERIORATION, SPALLING OR SCOUR HAVE SERIOUSLY AFFECTED PRIMARY STRUCTURAL COMPONENTS. LOCAL FAILURES ARE POSSIBLE. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT.

2 CRITICAL CONDITION - ADVANCED DETERIORATION OF PRIMARY STRUCTURAL ELEMENTS. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT OR SCOUR MAY HAVE REMOVED SUBSTRUCTURE SUPPORT. UNLESS CLOSELY MONITORED IT MAY BE NECESSARY TO CLOSE THE BRIDGE UNTIL CORRECTIVE ACTION IS TAKEN.

1 "IMMINENT" FAILURE CONDITION - MAJOR DETERIORATION OR SECTION LOSS PRESENT IN CRITICAL STRUCTURAL COMPONENTS OR OBVIOUS VERTICAL OR HORIZONTAL MOVEMENT AFFECTING STRUCTURAL STABILITY. BRIDGE IS CLOSED TO TRAFFIC BUT CORRECTIVE ACTION MAY PUT IT BACK IN LIGHT SERVICE.

0 FAILED CONDITION - OUT OF SERVICE AND BEYOND CORREC

| IDENTIFICATION | |
|---|---|
| (1) State Names | 47 - Tennessee |
| (8) Structure Number | 58100240069 |
| (5) Inventory Route | 1 |
| (2) Highway Agency District | Region 2 |
| (3) County Code | 58 - Marion |
| (4) Place Code | 00000 |
| (6) Features Intersected | I-24 WB / SHELLMOUND RD. |
| (7) Facility Carried | I24 WBL |
| (9) Location | 3 MI S OF SR28 |
| (11) Mile Point | 22.650 mi |
| (12) Base Highway Network | Yes |
| (13) LRS Inventory Rte & Subrte | 5810024001 |
| (16) Latitude | 35.043983 |
| (17) Longitude | -85.602900 |
| (98) Border Bridge State Code | |
| (99) Border Bridge Structure No. | |
| STRUCTURE TYPE AND MATERIAL | |
| (43) Main Structure Type | 24 |
| Material | 2 - Concrete continuous |
| Type | 4 - Tee beam |
| (44) Approach Structure Type | 00 |
| Material | 0 - Other / None |
| Type | 0 - Other / None |
| (45) No. of Spans in Main Unit | 3 |
| (46) No. of Approach Spans | 0 |
| (107) Deck Structure Type | 1 - Concrete Cast-in-Place |
| (108) Wearing Surface/Protective System | |
| Type of Wearing Surface | 6 - Bituminous |
| Type of Membrane | 0 - None |
| Type of Deck Protection | 0 - None |
| AGE AND SERVICE | |
| (27) Year Built | 1965 |
| (106) Year Reconstructed | 0 |
| (42) Type of Service | 11 |
| On | 1 - Highway |
| Under | 1 - Highway, with or without pedestrian |
| (28) Lane | |
| On | 2 |
| Under | 2 |
| (29) Average Daily Traffic | 52102 |
| (30) Year of ADT | 2021 |
| (109) Truck ADT | 7 % |
| (19) Bypass, Detour Length | 1 mi |
| GEOMETRIC DATA | |
| (48) Length of Maximum Span | 42.0 ft |
| (49) Structure Length | 106.0 ft |
| (50) Curb or Sidewalk Width | |
| Left | 0.0 ft |
| Right | 0.0 ft |
| (51) Bridge Roadway Width Curb to Curb | 36.2 ft |
| (52) Deck Width Out to Out | 40.3 ft |
| (32) Approach Roadway Width (W/Shoulders) | 24.0 ft |
| (33) Bridge Median | 1 - Open median |
| (34) Skew | 84 Deg |
| (35) Structure Flared | 0 - No flare |
| (10) Inventory Route Min Vert Clear | 99.99 ft |
| (47) Inventory Route Total Horiz Clear | 36.3 ft |
| (53) Min Vert Clear Over Bridge Rdwy | 99.99 ft |
| (54) Min Vert Underclear | 15.40 ft |
| Ref: | |
| (55) Min Lat Underclear RT | 10.0 ft |
| Ref: | |
| (56) Min Lat Underclear LT | 9.0 ft |
| NAVIGATION DATA | |
| (38) Navigation Control | N - Not applicable, no waterwa |
| (111) Pier Protection | |
| (39) Navigation Vertical Clearance | 0.0 ft |
| (116) Vert-Lift Bridge Nav Min Vert Clear | ft |
| (40) Navigation Horizontal Clearance | 0.0 ft |

| CLASSIFICATION | |
|--|--|
| (112) NBIS Bridge Length | Y |
| (104) Highway System | 1 |
| (26) Functional Class | 1 - Rural Principal Arterial - |
| (100) Defense Highway | 1 - The inventory route is on |
| (101) Parallel Structure | N - No parallel structure exis |
| (102) Direction of Traffic | 1 - way traffic |
| (103) Temporary Structure | |
| (105) Federal Lands Highways | 0 - N/A |
| (110) Designated National Network | 1 - The inventory route is par |
| (20) Toll | 3 - On free road. The structu |
| (21) Maintain | 1 - State Highway Agency |
| (22) Owner | 1 - State Highway Agency |
| (37) Historical Significance | 4 - Historical significance is |
| CONDITION | |
| (58) Deck | 6 |
| (59) Superstructure | 5 |
| (60) Substructure | 7 |
| (61) Channel & Channel Protection | N |
| (62) Culverts | N |
| LOAD RATING AND POSTING | |
| (31) Design Load | 5 - MS 18 / HS 20 |
| (63) Operating Rating Method | 8 |
| (64) Operating Rating | |
| Type | 8 - Load and Resistance Factor Rating (LRF |
| Rating | 33.70 |
| (65) Inventory Rating Method | 8 - Load and Resistance Factor |
| (66) Inventory Rating | |
| Type | |
| Rating | 25.92 |
| (70) Bridge Posting | 5 - Equal to or above legal loads |
| (41) Structure Open/Posted/Closed | A - Open, no restriction |
| APPRAISAL | |
| (67) Structural Evaluation | 5 |
| (68) Deck Geometry | 8 |
| (69) Clearances, Vertical/Horizontal | 5 |
| (71) Waterway Adequacy | N |
| (72) Approach Roadway Alignment | 8 |
| (36A) Bridge Railings | 1 - Inspected feature meets current |
| (36B) Transitions | 0 - Inspected feature does not meet |
| (36C) Approach Guardrail | 0 - Inspected feature does not meet |
| (36D) Approach Guardrail Ends | N - Not applicable or a safety feat |
| (113) Scour Critical Bridges | N - Bridge not over waterway. |
| PROPOSED IMPROVEMENTS | |
| (75) Type of Work | 35 - Bridge rehabilitation bec |
| (76) Length of Structure Improvement | 106.0 ft |
| (94) Bridge Improvement Cost | \$ 601 |
| (95) Roadway Improvement Cost | \$ 61 |
| (96) Total Project Cost | \$ 902 |
| (97) Year of Improvement Cost Estimate | 2021 |
| (114) Future ADT | 77918 |
| (115) Year of Future ADT | 2041 |

| INSPECTIONS * | | | |
|---|------------|-------------|------|
| (90) Inspection Date | 07/16/2024 | | |
| (91) Frequency | 24 | | |
| (92) Critical Feature Inspection | Done | Freq. (Mon) | Date |
| A: Fracture Critical Detail | No | | |
| B: Underwater Inspection | No | | |
| C: Other Special Inspection | No | | |
| <p>* The inspection date and frequency information in this box contains the current NBI date and frequency information. Please refer to the report header for the date this inspection was conducted.</p> | | | |



Looking ahead on route



Top of deck



Looking back on route



Typical bottom of deck



Left side view of structure



Right side view of structure



Underclearance looking ahead on underpass route



Underclearance looking back on underpass route



Bridge number and underpass number



Typical abutment



Typical bent



Typical spall in overhang



Collision damage to beams in span #2



Broken steel reinforcing in beam "A" in span #2



Vegetation growth between cracks in slope paving

PERFORMANCE EVALUATION

Time of Day Inspected 8:33 A.M.

Weather Conditions Clear, 90°F

Vehicles Observed All types

LIVE LOAD BEHAVIOR

Sub Horiz./ Vert. Defl (No)

Sub Vibration (No)

Super Horiz./ Vert. Defl (No)

Super Vibration (No)

APPROACH

Alignment (Good)

Slab (NA)

Joints (Good)

Pavement (Good)

Embankment (Good)

Approach Drains (NA)

TRAFFIC SAFETY FEATURES

Bridge Railing Rating (Good) Some minor cracks

Transitions Rating (Good)

Guardrail Rating (Good)

Guardrail Terminal Rating (Good)

SIGNS POSTED ON ROUTE

Paddleboards No

Weight Limit Posted Not Needed

Vertical Clearance (<14'-6") No

Gross Tons

Posted Height

Single-unit Vehicle Tons

Narrow Bridge Signs No

Multi-unit Vehicle Tons

One Lane Bridge Signs No

564 Assigned Bridge Name

Other Signs or Plaques

ATTACHED SIGNS

| Sign No | Location | Text on Sign | Noted Defects |
|---------|----------|--------------|---------------|
|---------|----------|--------------|---------------|

DECK

| Wearing Surface Type Asphalt | | Wearing Surface Depth 6 |
|------------------------------|--------|--|
| Wearing Surface | (Good) | |
| Deck - Structural Condition | (Fair) | Isolated minor and moderate spalls with exposed rebar in overhangs |
| Curbs | (NA) | |
| Median | (NA) | |
| Sidewalks | (NA) | |
| Parapet | (Good) | Some minor cracks |
| Railing | (NA) | |
| Rail Paint | (NA) | |
| Deck Drains | (NA) | |
| Lighting Standards | (NA) | |
| Utilities | (NA) | |
| Expansion Joints | (NA) | |

SUPERSTRUCTURE

| | | |
|------------------------|--------|--|
| Bearing Devices | (Good) | |
| Girders | (Fair) | Beams "A", "B", and "C" in span #2 have moderate collision damage; beam "A" in span #2 has severed reinforcing steel due to collision damage |
| Beams | (NA) | |
| Floor Beams | (NA) | |
| Stringers | (NA) | |
| Diaphragms | (Good) | Isolated minor cracks and delaminated areas |
| Superstructure Bracing | (NA) | |
| Trusses - General | (NA) | |
| Trusses - Portals | (NA) | |
| Trusses - Bracing | (NA) | |
| Superstructure Paint | (Good) | |
| Alignment of Members | (Good) | |

TEXTURE COAT

ABUTMENTS

| | | |
|--------------------------|---------------|----------------------------------|
| Abutment Caps | (Good) | Some minor cracking |
| Abutment Breastwall | (NA) | |
| Abutment Wings | (Good) | Minor spall at #2 right |
| Abutment Backwall | (Good) | |
| Abutment Plumb | (Good) | |
| Abutment Footing | (NA) | |
| Abutment Piles | (Not Visible) | |
| Abutment Embankment | (Good) | |
| Abutment Bearing Surface | (Good) | |
| Abutment Slope Paving | (Good) | Vegetation growing between slabs |
| Abutment Rip Rap | (NA) | |

PIERS

| | | |
|----------------------|------|--|
| Pier Caps | (NA) | |
| Pier Columns I Walls | (NA) | |
| Pier Plumb | (NA) | |
| Pier Footing | (NA) | |
| Pier Piles | (NA) | |
| Pier Bearing Surface | (NA) | |

BENTS

| | | |
|------------------------|---------------|---|
| Bent Caps | (NA) | |
| Bent Columns | (Good) | Some minor rebar pop-outs and delaminated areas |
| Bent Plumb | (NA) | |
| Bent Footing | (Not Visible) | |
| Bent Piles | (Not Visible) | |
| Bent Bearing Surface | (Good) | |
| Piles Need Replacement | (No) | |

Inspection Team's Summary

This bridge consists of three continuous concrete deck girder spans with an asphalt wearing surface which has a total length of 106 feet and a maximum span length of 42 feet. It is situated on a 84° left skew and crosses over Shell Mound Road. This structure was constructed in 1965. The bridge was inspected on July 16, 2024, by a Region 2 bridge inspection team from Tullahoma and was found to be in overall fair condition.

The approach roadway alignment is rated good. The approach pavement is rated good. The approach safety features are rated good but do not meet the current safety standards except for the terminal ends which do meet the current safety standards. The approach drains and embankments are rated good.

The deck is rated fair. The top of the deck is not visible due to the asphalt wearing surface. The bottom of the deck has isolated minor and moderate spalls in the overhangs. The concrete parapets are rated good and meet the current safety standards.

The superstructure is rated fair. All three concrete girders in span #2 have moderate spalled areas. Beam "A" has one severed reinforcing bar due to collision damage.

The substructure is rated good. The abutment caps have some minor cracking. The backwall of abutment #1 and a few bent columns have minor delaminated areas. The concrete slope pavement is rated good but has vegetation growth between the slabs. The bent columns have some minor rebar pop-outs and delaminated areas.

The underpass roadway is rated fair. There are currently no underpass safety protection features in place.

General Inspection Comment

HQ notes to TL

Deck Elevation

Benchmark height 644.59 Benchmark location Elevation "1" on top of curb, Edge location _____
on right side of abutment 1.







Comment

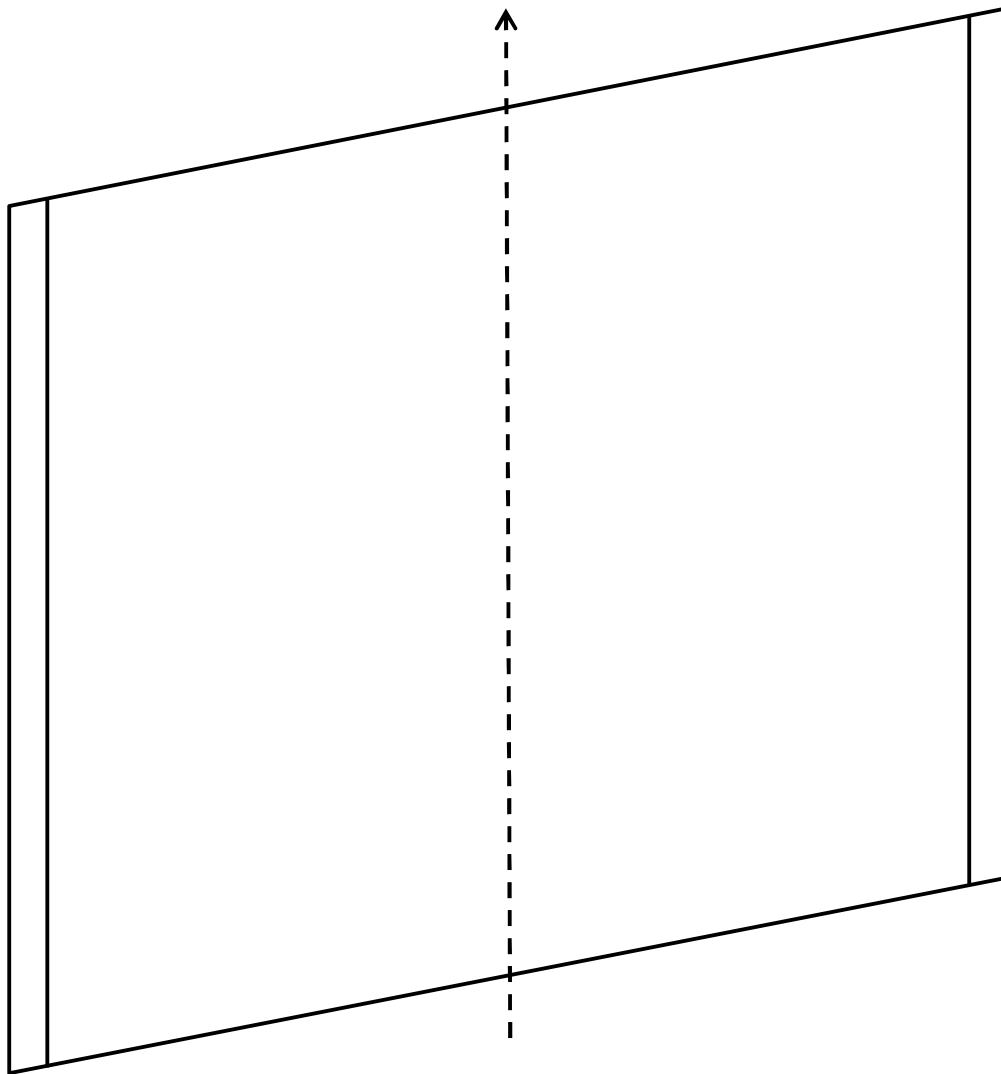
| Location | Top Lt. Curb | Left Gutter | Center Line | Right Gutter | Top Rt. Curb |
|------------|--------------|-------------|-------------|--------------|--------------|
| ABUTMENT 1 | 644.6 | 644.23 | | 643.77 | 644.22 |
| PIER 1 | 644.74 | 644.75 | | 643.06 | 644.41 |
| PIER 2 | 645.08 | 644.75 | | 644.36 | 644.7 |
| ABUTMENT 2 | 645.38 | 644.02 | | 644.36 | 644.85 |

Top of Deck Span No. 1

Date 07/16/24

Bridge Location No. 58 I-24 22.65 L
County Route Log Mile

| | | | | | |
|---------|--------------|---|-------------------------|---|-----------------------|
| Legend: | Cracking |  | Repairs |  | (light vertical) |
| | Spalling |  | Voids |  | (large checker board) |
| | Scaling |  | Dimensions are noted as | | |
| | Delamination |  | | | |
| | | (confetti) | | | |
| | | (10% dots) | | | |
| | | (40% gray) | | | |
| | | | ← X ↑ X | depth | |




Bottom of Deck Span No. 1

Date 07/16/24


Bridge Location No. 58 County I-24 Route 22.65 L Log Mile

Legend:

Cracking




Repairs




(light vertical)

Spalling




(confetti)

Voids



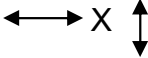
(large checker board)

Scaling




(10% dots)

Dimensions are noted as

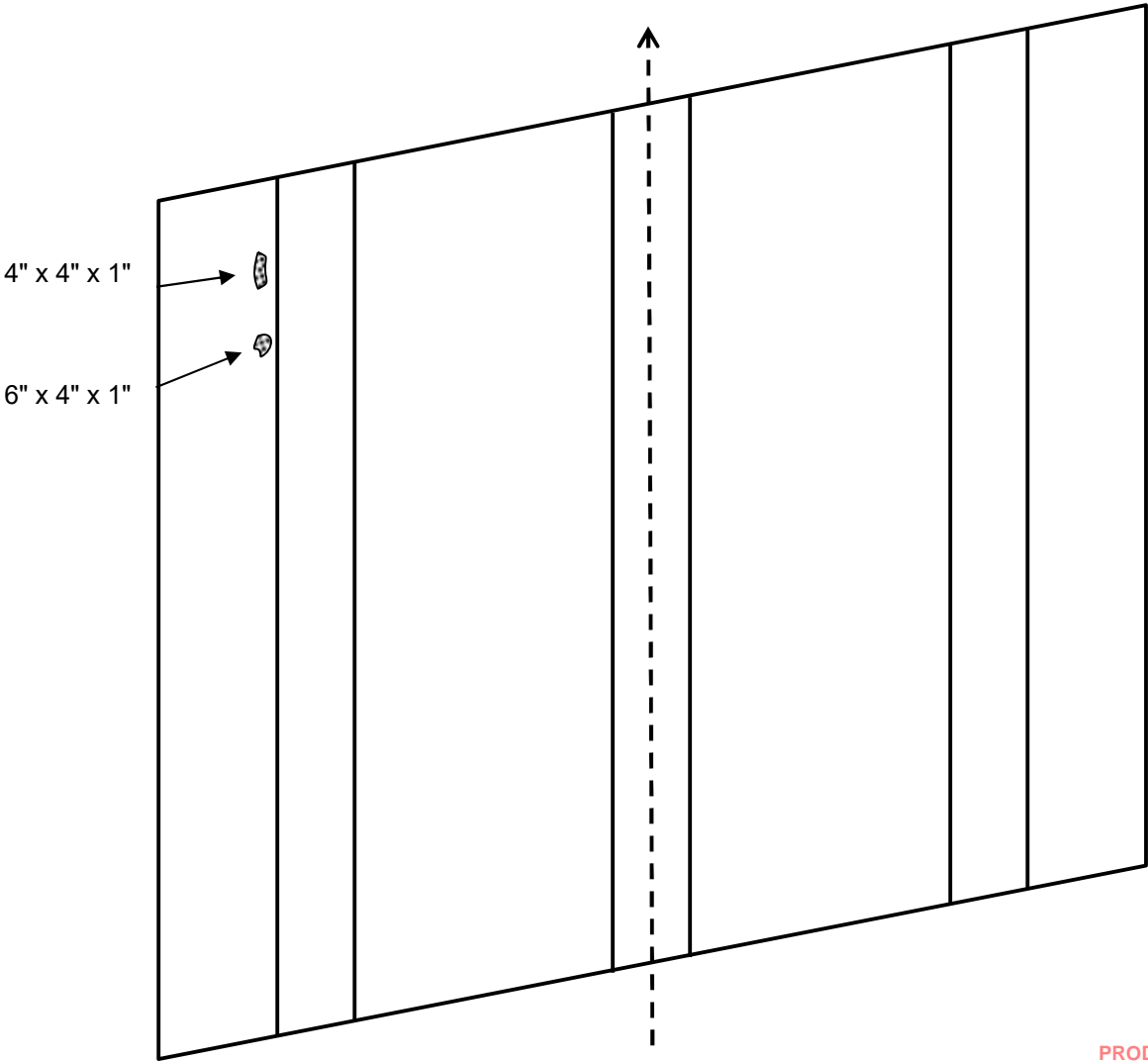
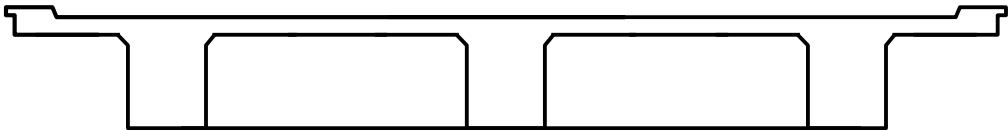


X X depth

Delamination









(40% gray)

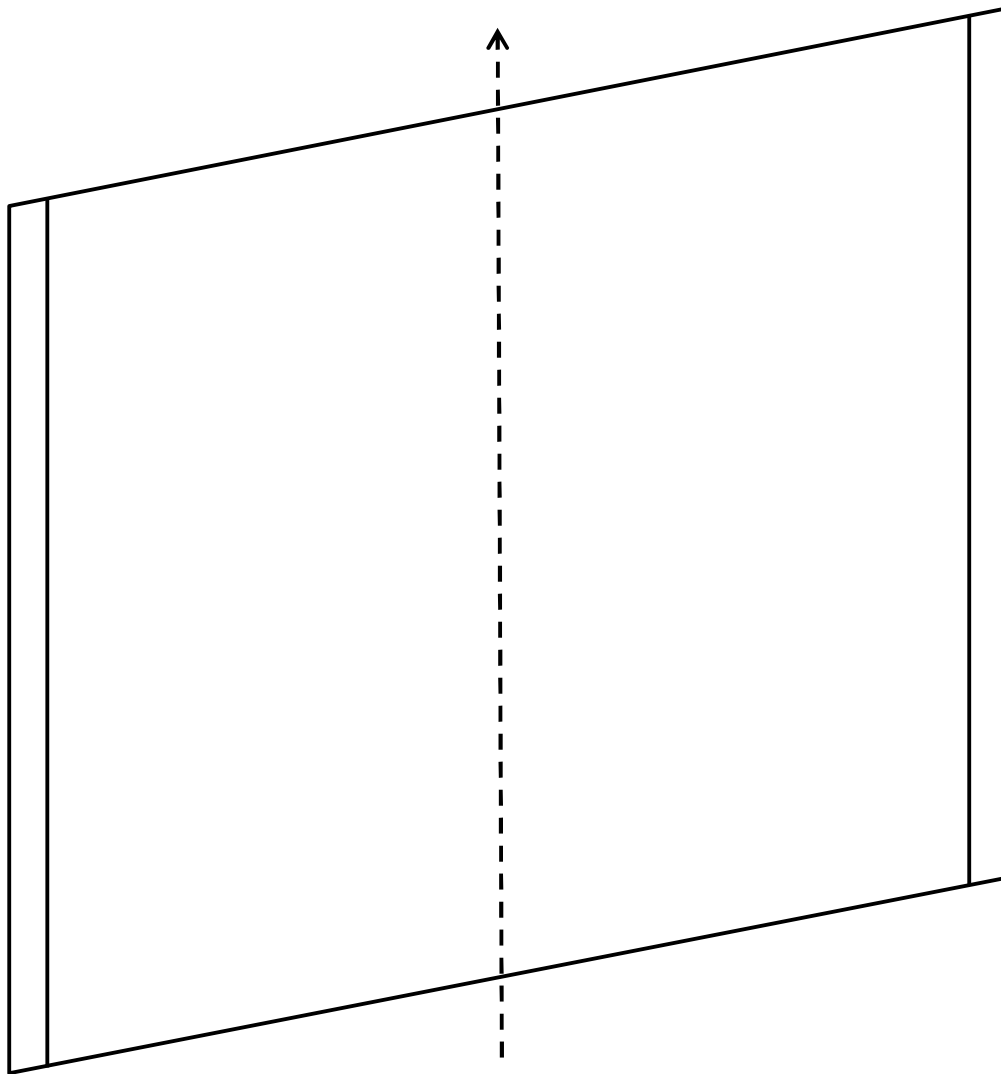


Top of Deck Span No. 2

Date 07/16/24

Bridge Location No. 58 I-24 22.65 L
County Route Log Mile

| | | | | | |
|---------|--------------|---|-------------------------|---|-----------------------|
| Legend: | Cracking |  | Repairs |  | (light vertical) |
| | Spalling |  | Voids |  | (large checker board) |
| | Scaling |  | Dimensions are noted as | | |
| | Delamination |  | | | |
| | | (confetti) | | | |
| | | (10% dots) | | | |
| | | (40% gray) | | | |
| | | | ← X ↑ X | depth | |




Bottom of Deck Span No. 2

Date 07/16/24


Bridge Location No. 58 County I-24 Route 22.65 L Log Mile

Legend:

Cracking




Repairs




(light vertical)

Spalling




(confetti)

Voids





(large checker board)

Scaling




(10% dots)

Dimensions are noted as

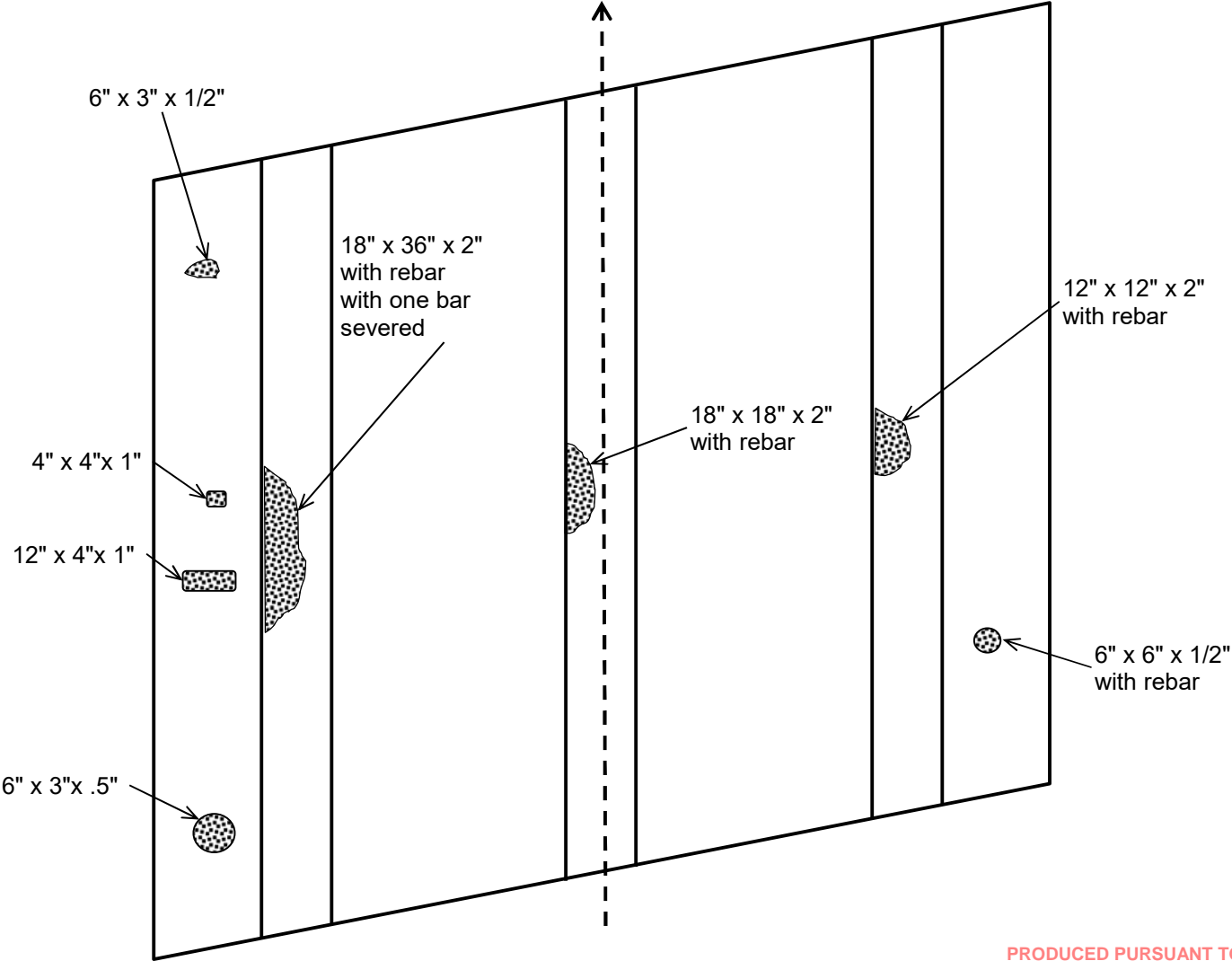
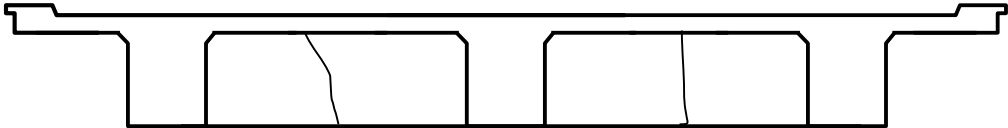
 X  X depth

Delamination



(40% gray)







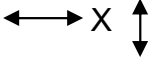
cracks are small

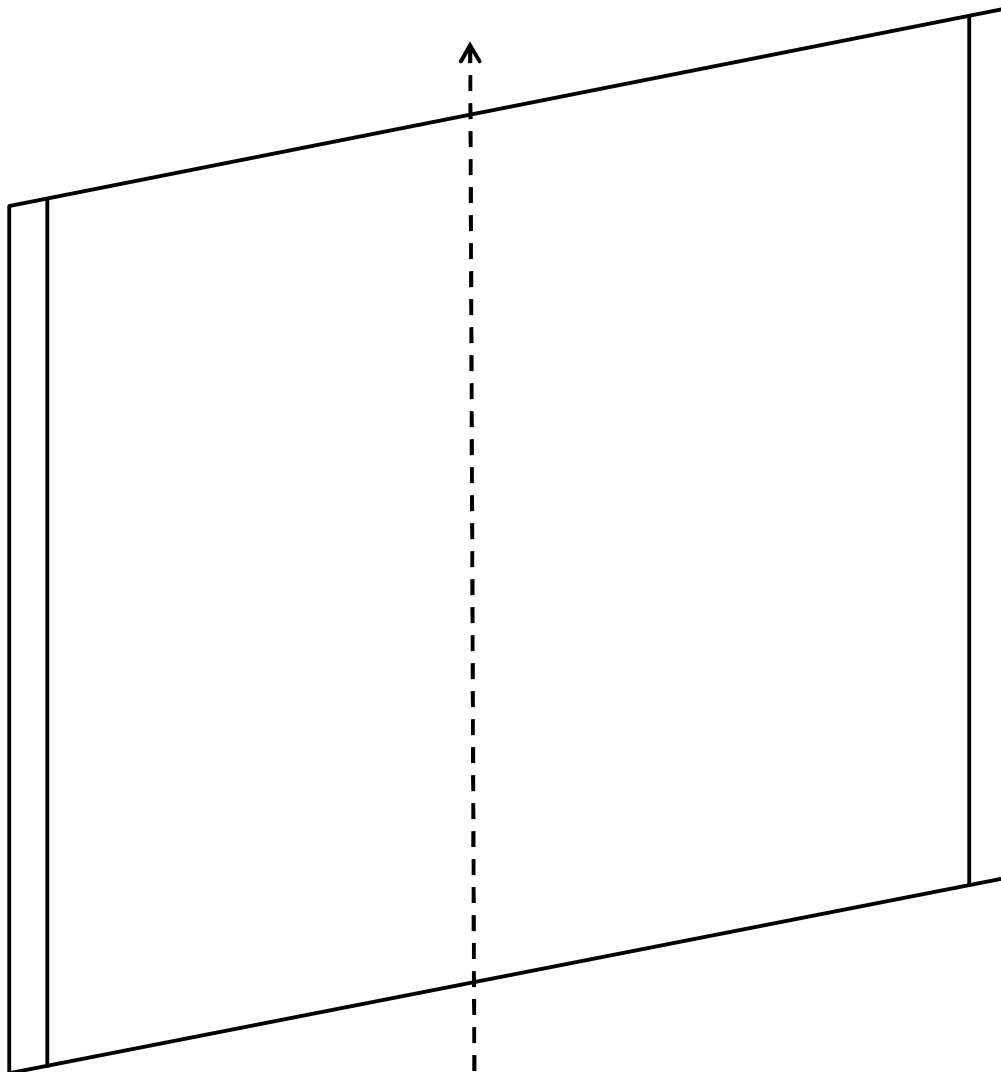


Top of Deck Span No. 3

Date 07/16/24

Bridge Location No. 58 I-24 22.65 L
County Route Log Mile

| | | | | | | |
|---------|--------------|---|------------|-------------------------|---|-----------------------|
| Legend: | Cracking |  | | Repairs |  | (light vertical) |
| | Spalling |  | (confetti) | Voids |  | (large checker board) |
| | Scaling |  | (10% dots) | Dimensions are noted as | | |
| | Delamination |  | (40% gray) | |  | X depth |



Bottom of Deck Span No. 3

Date 07/16/24

Bridge Location No. 58 County I-24 Route 22.65 L Log Mile

Legend:

Cracking

Repairs

(light vertical)

Spalling

(confetti)

Voids

(large checker board)

Scaling

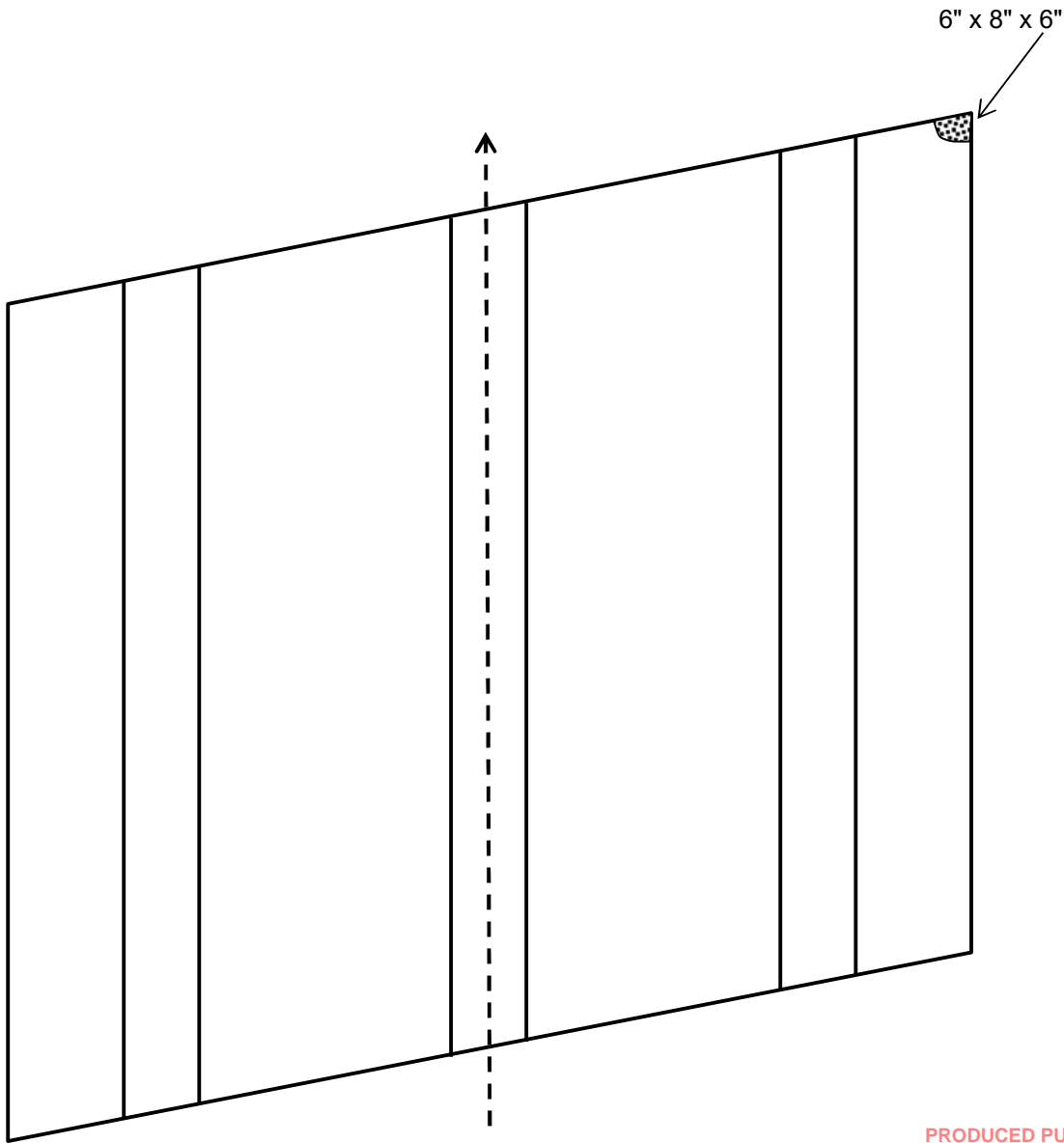
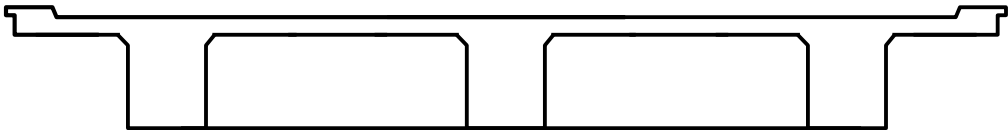
(10% dots)

Dimensions are noted as

X X depth

Delamination










(40% gray)

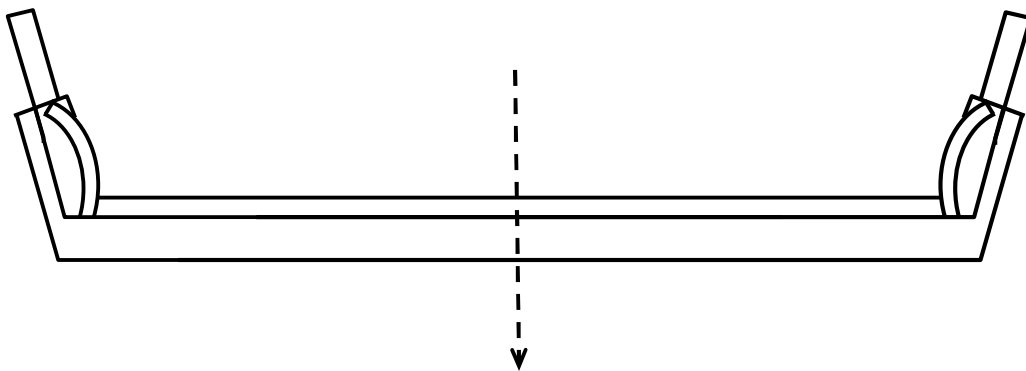


Abutment No. 1

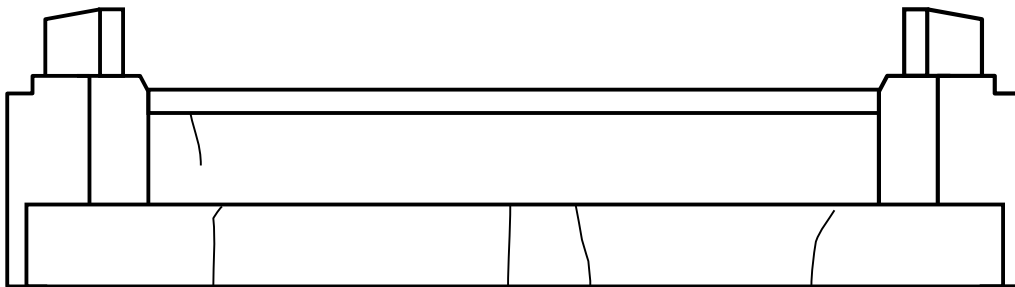
Date 07/16/24

Bridge Location No. 58 I-24 22.65 L
County Route Log Mile

| | | | | | | |
|---------|--------------|---|------------|---|---|---|
| Legend: | Cracking |  | | Repairs |  | (light vertical) |
| | Spalling |  | (confetti) | Bearings |  | (light upward diagonal) |
| | Scaling |  | (10% dots) | Voids |  | (large checker board) |
| | Delamination |  | (40% gray) | Dimensions are noted as | | |
| | | | |  | X |  X depth |












cracks are hairline

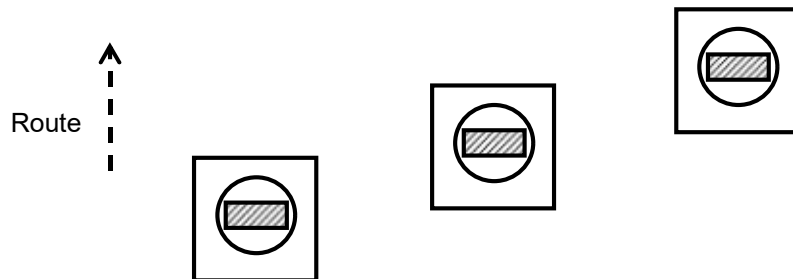


Bent No. 1

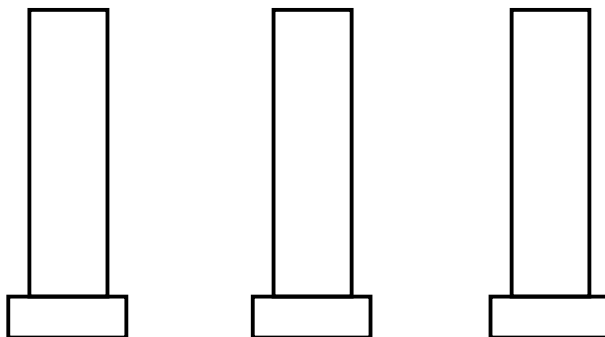
Date 07/16/24

Bridge Location No. 58 I-24 22.65 L
County Route Log Mile

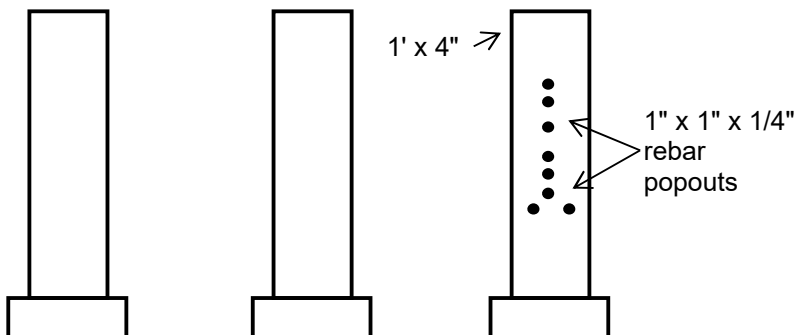
| | | | | | | |
|---------|--------------|---|------------|---|---|-------------------------|
| Legend: | Cracking |  | | Repairs |  | (light vertical) |
| | Spalling |  | (confetti) | Bearings |  | (light upward diagonal) |
| | Scaling |  | (10% dots) | Voids |  | (large checker board) |
| | Delamination |  | (40% gray) | Dimensions are noted as | | |
| | | | |  X  X depth | | |



Front Side












Back Side

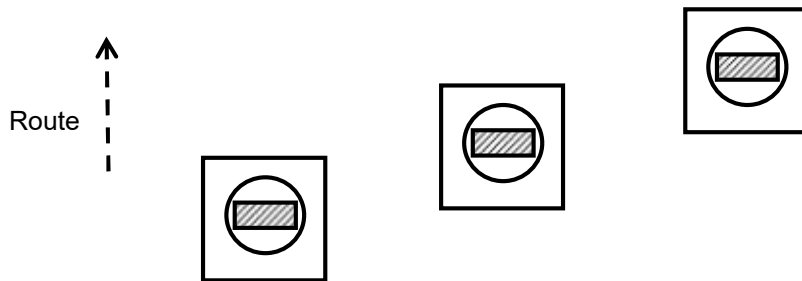


Bent No. 2

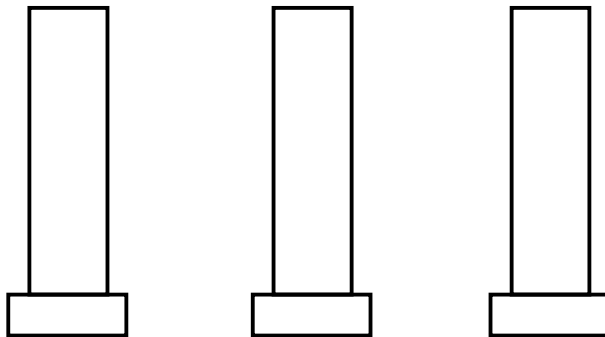
Date 07/16/24

Bridge Location No. 58 I-24 22.65 L
County Route Log Mile

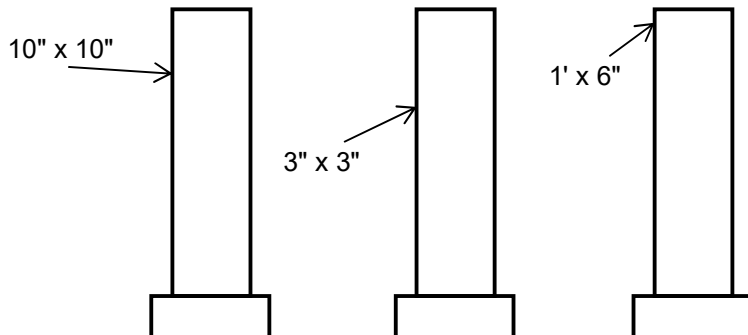
| | | | | | |
|---------|--------------|---|---|---|---|
| Legend: | Cracking |  | Repairs |  | (light vertical) |
| | Spalling |  | Bearings |  | (light upward diagonal) |
| | Scaling |  | Voids |  | (large checker board) |
| | Delamination |  | Dimensions are noted as | | |
| | | |  | X |  X depth |



Front Side












Back Side

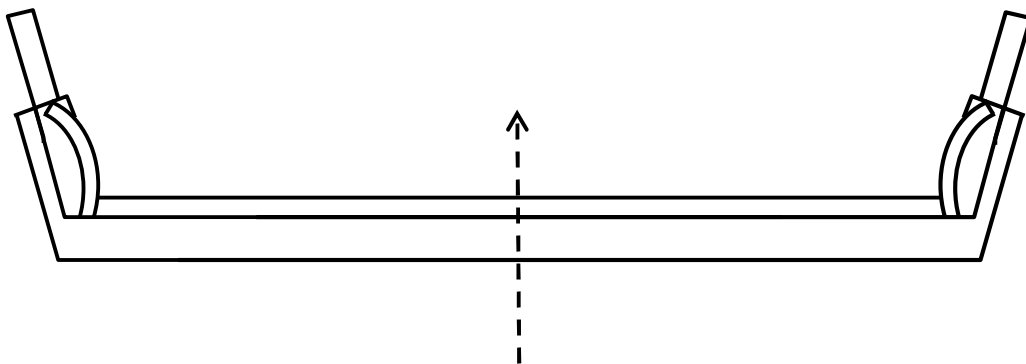


Abutment No. 2

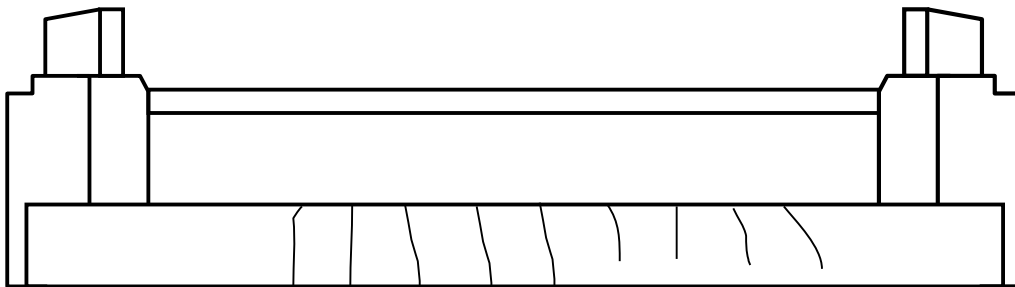
Date 07/16/24

Bridge Location No. 58 I-24 22.65 L
County Route Log Mile

| | | | | | | |
|---------|--------------|---|------------|---|---|-------------------------|
| Legend: | Cracking |  | | Repairs |  | (light vertical) |
| | Spalling |  | (confetti) | Bearings |  | (light upward diagonal) |
| | Scaling |  | (10% dots) | Voids |  | (large checker board) |
| | Delamination |  | (40% gray) | Dimensions are noted as | | |
| | | | |  X  X depth | | |



cracks are hairline



Form BIR 3.10

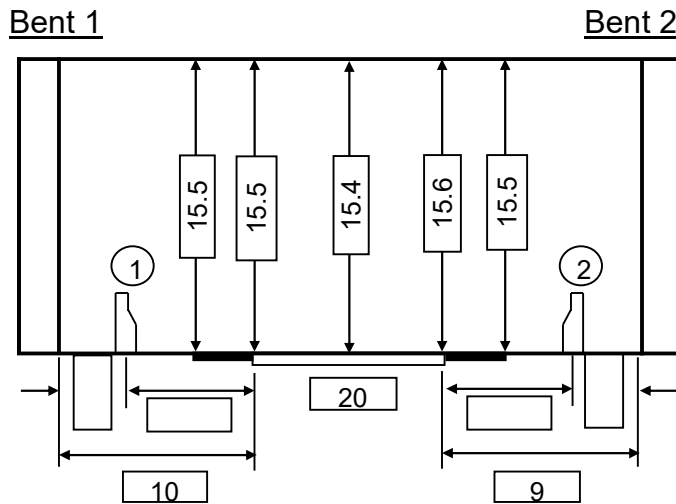
REVISED 6-9-92

Date 07/16/24

Bridge Location No. 58 I-24 22.65 L
County Route Log Mile

NOTE: ALL DISTANCES AND
ELEVATIONS ARE IN FEET.

Lateral and Vertical Clearances for One Lane Highway



1. Rail / Barrier Type:

W-Shape

☐
☐

Conc. Barrier

☐
☐

None

☒
☐

2. Rail / Barrier Type:

W-Shape

☐
☐

Conc. Barrier

☐
☐

None

☒
☐

Equipment List

General Inspection

☒ Yes Pocket knife
☒ Yes Sounding/chipping hammer
☐ Chain drag
☒ Yes Range pole
☐ 25' rod - depth and clearance

Visual Aid

☐ Binoculars
☐ Flashlight
☐ Magnifying glass
☐ Hand mirror

Cleaning

☐ Wisk broom
☐ Wire brush
☐ Flat bladed screwdriver
☐ Hand shovel
☐ Penetrating oil (WD-40, etc.)

Tools For Access

☐ Ladders
☐ Rope
☐ Waders
☐ Machete or bush axe

Comment

Tools For Measuring

☐ Masonry/Wood Ruler
☒ Yes 6' Pocket Tape
☐ 25' and 100' Tape
☐ Calipers
☐ Thermometer
☐ Carpenter's Level
☐ String and Weighted line (plumb bob)

Special Purpose Equipment

☐ Reach All
☐ Bucket Truck
☐ Traffic control
☐ Boat
☐ Sonar depth finder
☐ Increment borer
☐ Survey equipment
☐ Safety Harness
☐ Climbing equipment
☐ Dye penetrant
☐ Drone
☐ Air Meter

Special Purpose Equipment

Reach-All Approval and Comments

| ELEMENTS | DESCRIPTION | UNITS | TOTAL | CS1 | CS2 | CS3 | CS4 |
|---|---------------------------------|-------|--------|--------|-----|-----|-----|
| 16 | Re Conc Top Flange | SF | 4311 | 4303 | 6 | 2 | 0 |
| 1080 | Delamination/Spall/Patched Area | SF | 8 | 0 | 6 | 2 | 0 |
| 510 | Wearing Surfaces | SF | 3842.5 | 3842.5 | 0 | 0 | 0 |
| (16) Element record added 2016-07-25. | | | | | | | |
| (1080-16) Element record added 7/20/2020 | | | | | | | |
| (510-16) Element record added 2016-07-25. | | | | | | | |
| 110 | Re Conc Opn Girder/Beam | LF | 318 | 311 | 1 | 6 | 0 |
| 1080 | Delamination/Spall/Patched Area | LF | 4 | 0 | 1 | 3 | 0 |
| 1090 | Exposed Rebar | LF | 3 | 0 | 0 | 3 | 0 |
| 1130 | Cracking (RC and Other) | LF | 19 | 19 | 0 | 0 | 0 |
| (110) Element record added 2016-07-25. | | | | | | | |
| (1080-110) Element record added 7/20/2020 | | | | | | | |
| (1090-110) Element record added 7/20/2020 | | | | | | | |
| 205 | Re Conc Column | EA | 6 | 2 | 4 | 0 | 0 |
| 1080 | Delamination/Spall/Patched Area | EA | 3 | 0 | 3 | 0 | 0 |
| 1090 | Exposed Rebar | EA | 1 | 0 | 1 | 0 | 0 |
| (205) Element record added 2016-07-25. | | | | | | | |
| (1080-205) Element record added 7/20/2020 | | | | | | | |
| (1090-205) Element record added 7/20/2022 | | | | | | | |
| 215 | Re Conc Abutment | LF | 83 | 83 | 0 | 0 | 0 |
| (215) Element record added 2016-07-25. | | | | | | | |
| 310 | Elastomeric Bearing | EA | 6 | 6 | 0 | 0 | 0 |
| (310) Element record added 2016-07-25. | | | | | | | |
| 331 | Re Conc Bridge Railing | LF | 212 | 212 | 0 | 0 | 0 |
| 1130 | Cracking (RC and Other) | LF | 15 | 15 | 0 | 0 | 0 |
| (331) Element record added 2016-07-25. | | | | | | | |